

Transport and Environment Committee

10.00am, Tuesday, 21 March 2017

Objections to Traffic Regulation Order TRO/13/45 - Greenways' Parking Places Charges

Item number	7.9
Report number	
Executive/routine	Executive
Wards	6 – Corstorphine/Murrayfield 7 – Sighthill/Gorgie 9 – Fountainbridge/Craiglockhart 10 – Meadows/Morningside 11 – City Centre 12 – Leith Walk

Executive Summary

At its meeting on [27 August 2013](#), the Transport and Environment Committee approved a report to commence the statutory process to introduce parking charges in Greenways' parking places within the CPZ and approve the pilot of a cashless only payment service in these areas.

Traffic Regulation Order (TRO/13/45) was advertised between 18 March 2016 and 12 April 2016 and two objections were received.

The purpose of this report is to address these objections, recommending Committee makes a change to the proposals based on one objection and sets aside the other objection. This change will enable the introduction of parking charges in Greenways' parking places.

Links

Coalition Pledges

Council Priorities [CP11](#), [CP12](#)

Single Outcome Agreement [SO4](#)

Objections to Traffic Regulation Order TRO/13/45 - Greenways Parking Places Charges

1. Recommendations

- 1.1 It is recommended that Committee:
 - 1.1.1 notes the objections received;
 - 1.1.2 agrees to convert free parking places on Ardmillan Terrace to shared use parking places to enable residents to purchase S4 parking permits allowing them to park in the bays outside their homes;
 - 1.1.3 sets aside an objection regarding Slateford Road; and
 - 1.1.4 proceeds to make the Traffic Regulation Order as advertised.

2. Background

- 2.1 The aim of Greenways' in Edinburgh is to ensure fast and reliable public transport services to and from the city centre during peak times. This encourages people to travel by bus and reduces congestion and pollution in the city whilst also improving accessibility for cyclists, motorcyclists and taxis.
- 2.2 There are five Greenway routes in Edinburgh, these include:
 - Lothian Road and Tollcross;
 - Dalry Road, Gorgie Road and Calder Road;
 - Glasgow Road and Corstorphine Road (A8);
 - Leith Walk and Leith Street; and
 - Slateford Road.
- 2.3 Outwith peak times, parking or loading activities can take place in Greenway areas free of charge. However, parking charges apply on the surrounding streets and on other main traffic routes within the Controlled Parking Zone (CPZ) which are not designated as Greenway. This approach is inconsistent, inequitable, creates confusion for motorists and results in further parking problems, such as poor accessibility caused by a low turnover of spaces.
- 2.4 The aim of the proposals is to ensure a consistent charging approach across the CPZ, to manage parking demands better and to tackle parking problems, such as commuter parking which prevents short-term parking opportunities for people visiting shops, businesses and local services.

3. Main report

- 3.1 The Transport and Environment Committee, on 27 August 2013, approved the commencement of the necessary statutory process to introduce parking charges in Greenways' parking places within the CPZ and to operate a cashless only payment service in such areas.
- 3.2 The draft Traffic Regulation Order (TRO/13/45) was advertised between 18 March 2016 and 12 April 2016.
- 3.3 On-street notices were placed near to the places affected, adverts were put in the local press and documents were made available for inspection at the City Chambers. Additionally, the proposals were advertised on the Council's website and on Scotland's Public Information Notice portal; www.tellmesotland.gov.uk.
- 3.4 At the end of the consultation period two objections were received.

Slateford Road

- 3.5 One objection regarded free parking places on Slateford Road, between Harrison Road and Laurel Terrace, outside St Michael's Parish Church.
- 3.6 Concerns were raised that parking charges would be detrimental to the functioning of the church and negatively impact on: funerals; weddings; and daytime community activities. It was argued that many users benefit from the free parking facilities available adjacent to the church and that people marking significant occasions in their lives should not have to consider the cost of their parking.
- 3.7 The Greenways' parking places on Slateford Road are currently available for parking during the day (9.30am to 4.00pm, Monday to Friday) to any motorists on a first come, first served basis and are free of charge. This means that motorists can park, without time limit, for any purpose and they are not for the exclusive use of the church.
- 3.8 It is proposed to introduce parking charges to ensure that: parking opportunities are available throughout the day for all road users; controls are equitable with those in the surrounding streets; and on-street parking is provided in accordance with Council transport policies.
- 3.9 The Council has a duty to reduce carbon emissions and improve air quality in the city centre, of which motor vehicle traffic is a significant contributory factor. The introduction of charges across the Greenways' parking places within the extent of the CPZ has the potential to reduce unnecessary vehicle journeys, cut emissions and enhance air quality.
- 3.10 The availability of free parking is likely to encourage people to drive rather than walk, cycle or use public transport to travel to their destinations. Public parking spaces on main routes are subject to high demand and resolving the current anomaly will help to ensure spaces are available throughout the day.
- 3.11 The introduction of parking charges will not prevent anyone from driving to or parking in the spaces, but they will address long-term parking problems and encourage people to consider their travel options.

- 3.12 The proposals will not impact on disabled persons' blue badge holders who will continue to be able to park free of charge at times when parking is permitted.
- 3.13 The impact of parking charges will be limited on Slateford Road as they will only apply Monday to Friday, similar to the parking places in the surrounding CPZ areas and do not extent beyond Weston Gait. Therefore, free parking would be available in other parking places on Slateford Road.

Ardmillan Terrace

- 3.14 The other objection was received from a resident of Ardmillan Terrace who was concerned about where residents would be able to park should charges be introduced in the free parking places outside their homes.
- 3.15 There were also concerns about increasing parking demands on the spaces from visitors using nearby facilities in the area, namely: two dental surgeries; a medical centre; a hotel; local businesses; and new residential developments under construction.
- 3.16 Currently, residents are able to use the free parking places to park outside their homes. However, with the introduction of parking charges and maximum stay periods these conditions will effectively prevent residents from parking in these areas throughout the day.
- 3.17 It is not the aim of the proposals to make parking more difficult for residents. Therefore, it is proposed that Greenway parking places on Ardmillan Terrace be converted to shared use parking places.
- 3.18 Shared use parking will provide the flexibility to allow residents' permit holders to park outside their homes while at the same time managing the expected increased demands on the spaces from visitors parking when using the local facilities.
- 3.19 The introduction of parking charges in these areas will also trial the use of cashless only payments through [RingGo](#), the Council's cashless parking provider. The outcomes of this trial will feed into any further changes to on-street parking payment facilities, such as the possible use of contactless card technology.

4. Measures of success

- 4.1 Success will be measured by:
- Managing the use of Greenways' parking places in the city centre through enforcement procedures and improving compliance;
 - Making parking charges fairer and easier for motorists to understand; and
 - Reducing the negative impacts of commuter parking and thereby improving parking opportunities around local shops and businesses.

5. Financial impact

- 5.1 The introduction of parking charges at the locations concerned is expected to cost around £10,000. This includes Traffic Regulation Order costs, promotional materials and the amendment or introduction of new signs. These costs will be met from within the 2017/18 parking revenue budget.
- 5.2 The previous report to Committee in August 2013 estimated that pay and display income from these parking places may be in the region of £150K per annum. While elasticity of parking demand is difficult to predict, projected income is likely to be lower as a result of changes to Ardmillan Terrace and the possible reduction of parking places on West Coates and Leith Walk to accommodate other transport improvement projects. However, income from spaces in surrounding streets may increase as a result.

6. Risk, policy, compliance and governance impact

- 6.1 The aims of this proposal align well with the Transport 2030 Vision, Local Transport Strategy 2014-19, the Parking Action Plan and the Air Quality Action Plan.

7. Equalities impact

- 7.1 The proposals will help to enhance parking opportunities for shoppers, visitors, businesses and their customers, by tackling commuter and long-term parking patterns. This change will benefit disabled persons' blue badges holders, improving their accessibility, as they will continue to be able to park free of charge while other motorists will be subject to control measures. Currently, disabled persons' blue badges provide no benefit to their holders in Greenways' places as anyone can park for free and without limit of time.
- 7.2 A cashless only payment service for Greenways' parking places may have an impact on some disabled people who do not qualify for a blue badge. People with learning difficulties, dyscalculia or other impairments which could cause them difficulty while using a mobile device may be at a disadvantage.
- 7.3 However, the existing ticket machines and traditional coin payment methods could equally be disadvantageous, as for some people handling coins and affixing a voucher to their vehicles windscreen may be more problematic than paying using a mobile device.
- 7.4 Another concern regards language, as people from some backgrounds are less likely to speak English, speak it well or only use it as a second language. Communicating the need to pay for parking by phone could be difficult. However, RingGo offers information in alternative languages on its website/app and this could be more accessible for visitors or non-English speakers than currently paying with coins where the only information available is provided in English.

- 7.5 Other socio-economic factors which may be relevant in terms of inequality regards those who do not have either a mobile phone, bank account or a credit card. It is considered that only in extremely rare circumstances someone would not have all three and still be able to run a vehicle. However, where available, motorists could still use a nearby ticket machine to pay for their parking time.
- 7.6 Mobile phone ownership in Scotland has reached 91% and more locally 75% of Edinburgh's residents own a smart phone. Cashless payment for goods and services is becoming more prevalent throughout the country and the same is true for parking payments in Edinburgh. The UK government has also helped launch fee-free bank accounts specifically for people on low-incomes. Furthermore, most wages and benefits are now paid directly into the recipients' bank account.
- 7.7 The potential impacts surrounding cashless parking have been discussed with the Edinburgh Access Panel and the trial in Greenways parking places will help to identify any other potential issues associated with the service.

8. Sustainability impact

- 8.1 The impacts of this report in relation to the three elements of the Climate Change (Scotland) Act 2009 Public Bodies Duties have been considered, and the outcomes are summarised below.
- 8.2 The proposals in this report will reduce carbon emissions because parking charges make it less attractive for people to travel by private car and park in the city centre.
- 8.3 The proposals in this report will increase the city's resilience to climate change impacts because they will make it less attractive for people to travel by car.
- 8.4 The impacts of this report in relation to the three elements of the Climate Change (Scotland) Act 2009 Public Bodies Duties have been considered, and the outcomes are summarised below.
- The proposals in this report will help achieve a sustainable Edinburgh because they will tackle commuter parking and improve accessibility for other road users.
 - The proposals in this report will help enhance access to local businesses and services. Commuter and long-term parking adds little value to the economy of an area.
 - The proposals in this report are not considered to negatively impact on environmental good stewardship as the use of natural resources will not be directly impacted.

9. Consultation and engagement

- 9.1 The previous report on this matter in 2013 detailed the results of the consultation activities up to that time.

- 9.2 The report also approved the commencement of the appropriate Traffic Regulation Order procedure to make the necessary changes.
- 9.3 Part of this process includes a public consultation and details of this have been advertised in the press, on the Council's website, on Scotland's Public Information Notice portal (www.tellmesotland.gov.uk) and documents were made available at the City Chambers.
- 9.4 Two objections were received to the proposals. This report recommends an approach that will set aside one of the objections and accommodate the other.

10. Background reading/external references

- 10.1 Report to the Transport and Environment Committee on 27 August 2013 entitled '[Parking Charges on Greenways within the Controlled Parking Zone.](#)'

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11. Links

Coalition Pledges

Council Priorities

CP11 - An accessible connected city

CP12 - A built environment to match our ambition

Single Outcome Agreement

SO4 - Edinburgh's communities are safer and have improved physical and social fabric

Appendices

1 - Maps of Greenways' Parking Places.

Appendix 1: Greenways Parking Places Key



Light Green – Greenways Limited Waiting Parking Places



Dark Green (also shown as dark green border with yellow fill on smaller scale maps) – Greenways Loading Places



Royal Blue – Public Parking Places



Pink – Permit Holders Only Parking Places



Brown – Shared Use Parking Places (can be used by permit holders or Pay and Display users)



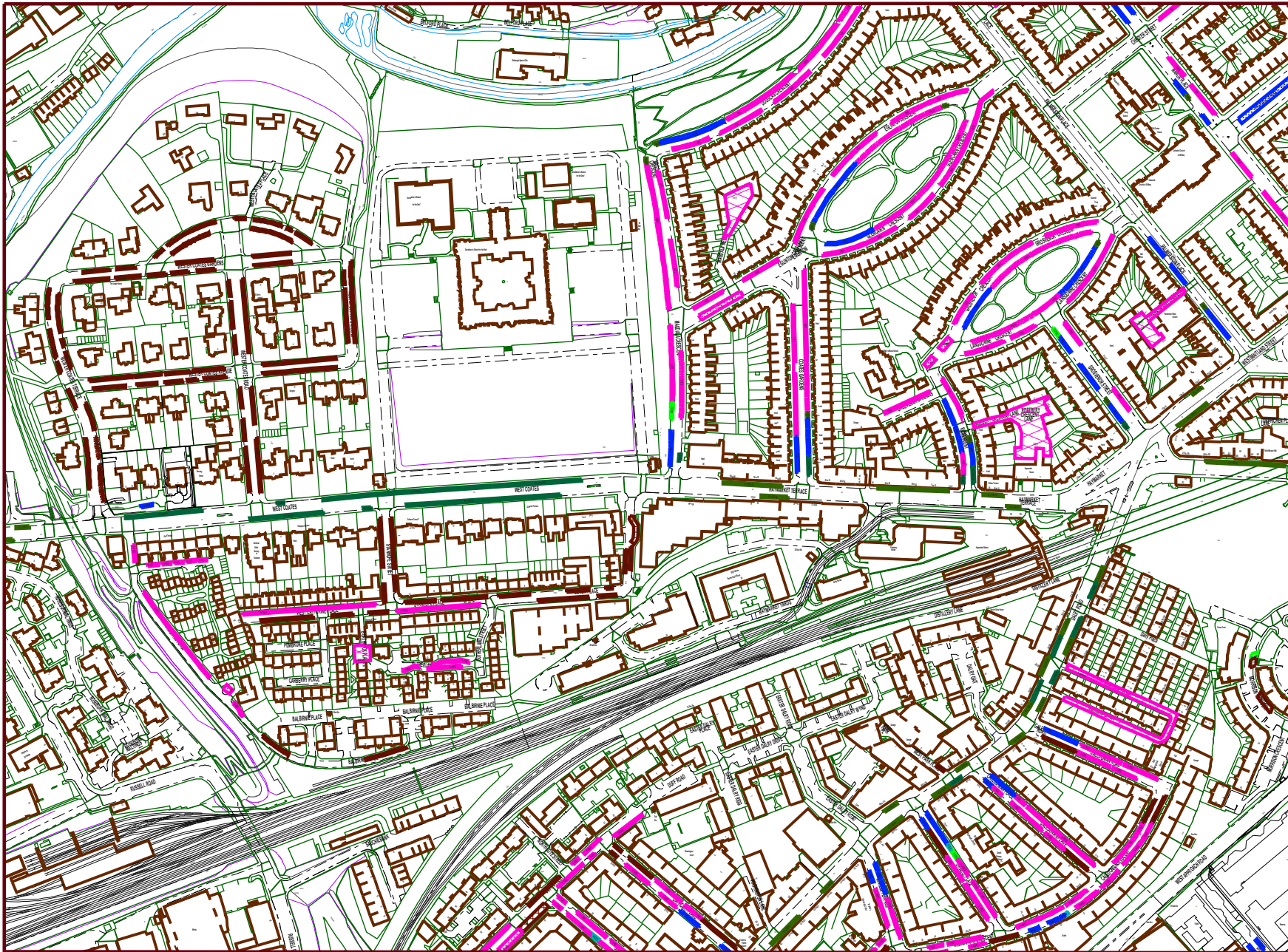
Bright Green – City Car Club Parking Places



Light Blue – Disabled Persons' Parking Place



Orange – Loading Place



NOTES

Dark green: Greenways public parking places.

Public parking places are located on: West Coates along with parts of Coates Gardens, Magdala Crescent, Rosebery Crescent and Grosvenor Street.



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 4 East Market Street, Edinburgh EH8 8DN
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Greenways Public Parking Places A8

1:5000

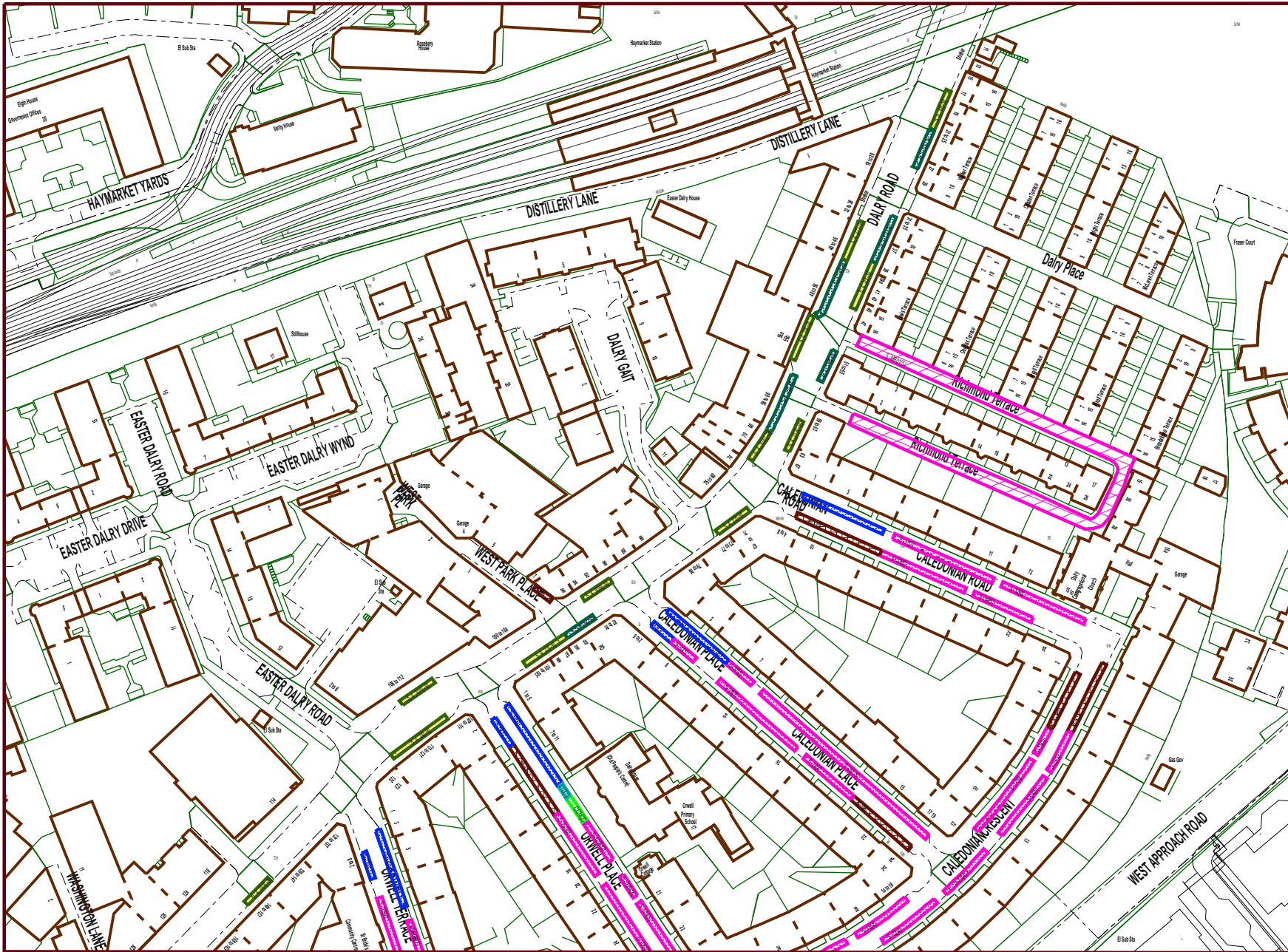


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NOTES

Dark green: Greenways public parking places.

Streets included: Dalry Road.



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Greenways Gorgie Dalry Public Parking Places 1

1:2000

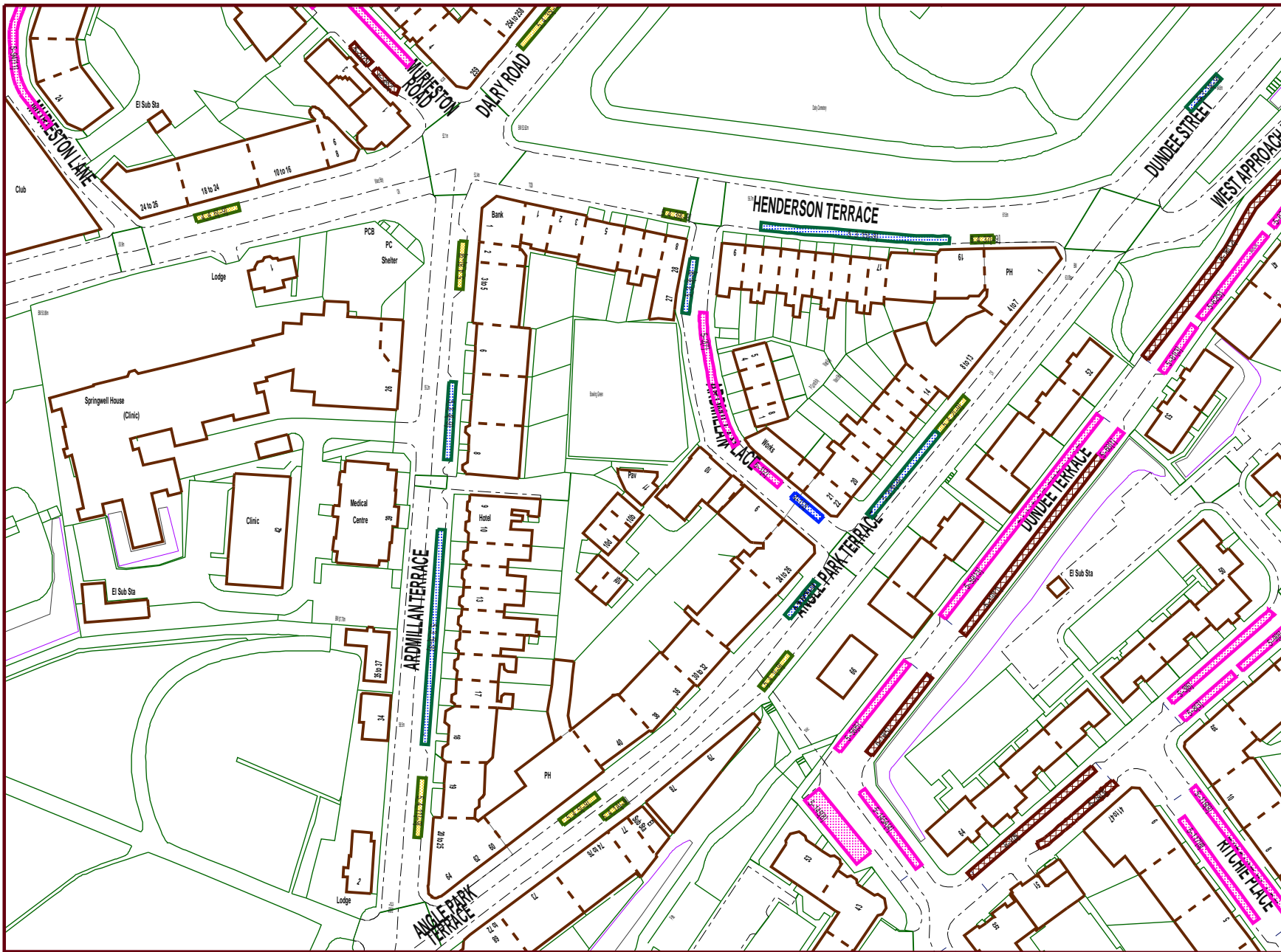


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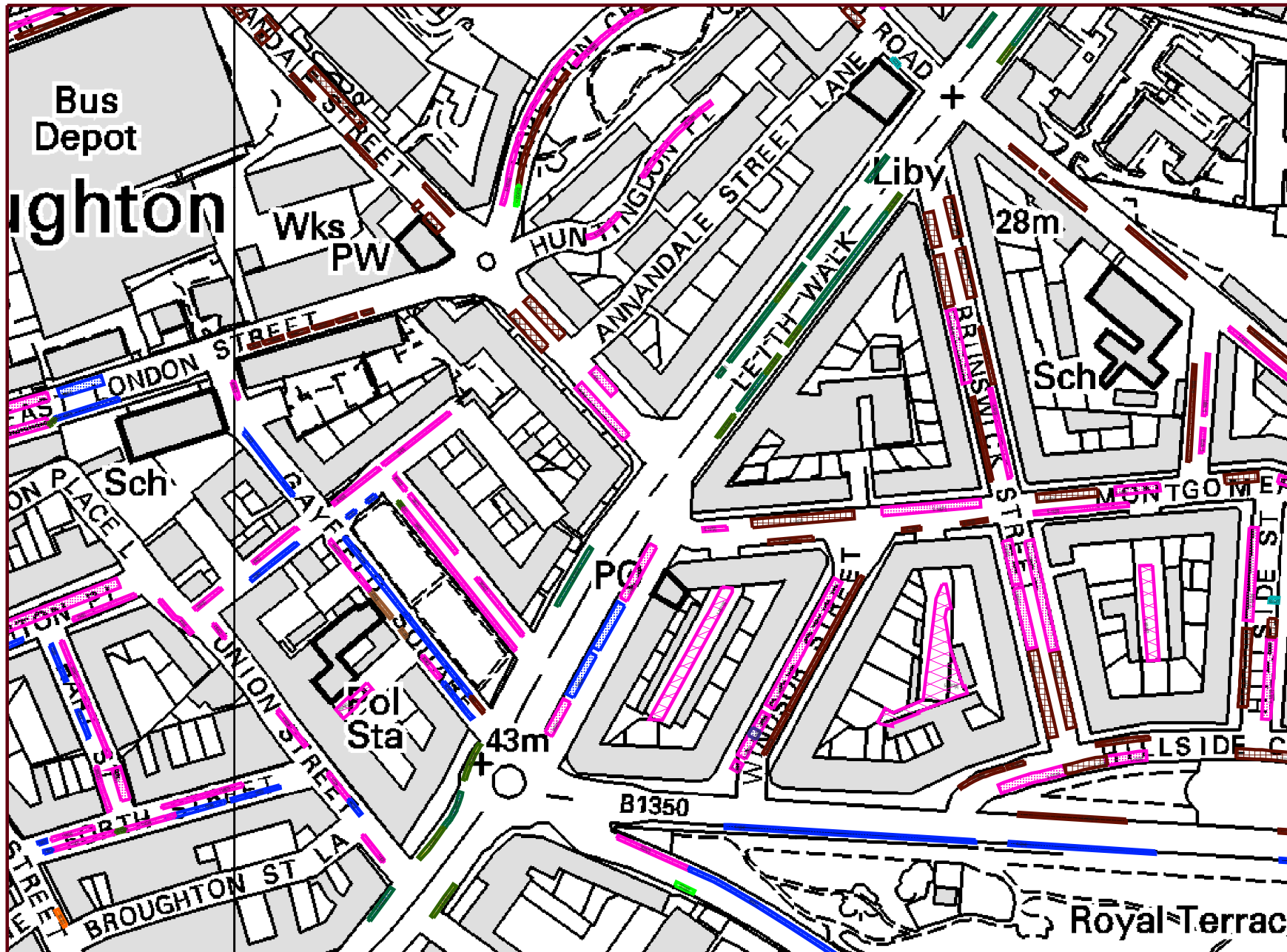


NOTES

Dark green: Greenways public parking places.

Streets included: Ardmillan Terrace, Ardmillan Place and Henderson Terrace.

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NOTES

Dark green: Greenways public parking places

Streets included: Leith Walk



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Greenways Leith Walk Public Parking Places

1:3000

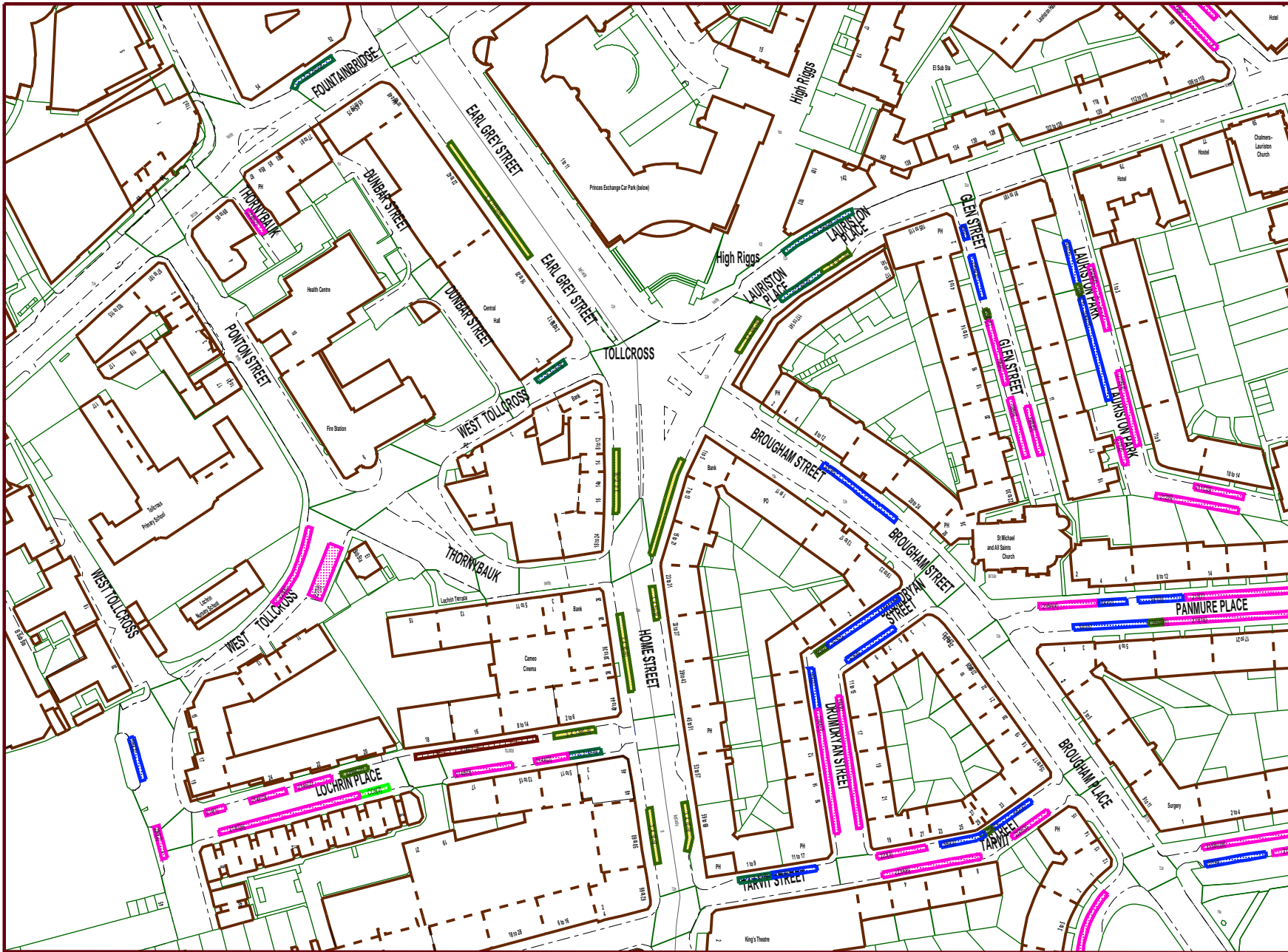
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NOTES

Dark green: Greenways public parking places.

Streets included: Fountainbridge, West Tollcross, Lauriston Place, Lochrin Place and Tarvit Street.



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Greenways Lothian Road and Tollcross Public Parking
 Places 1

1:2000

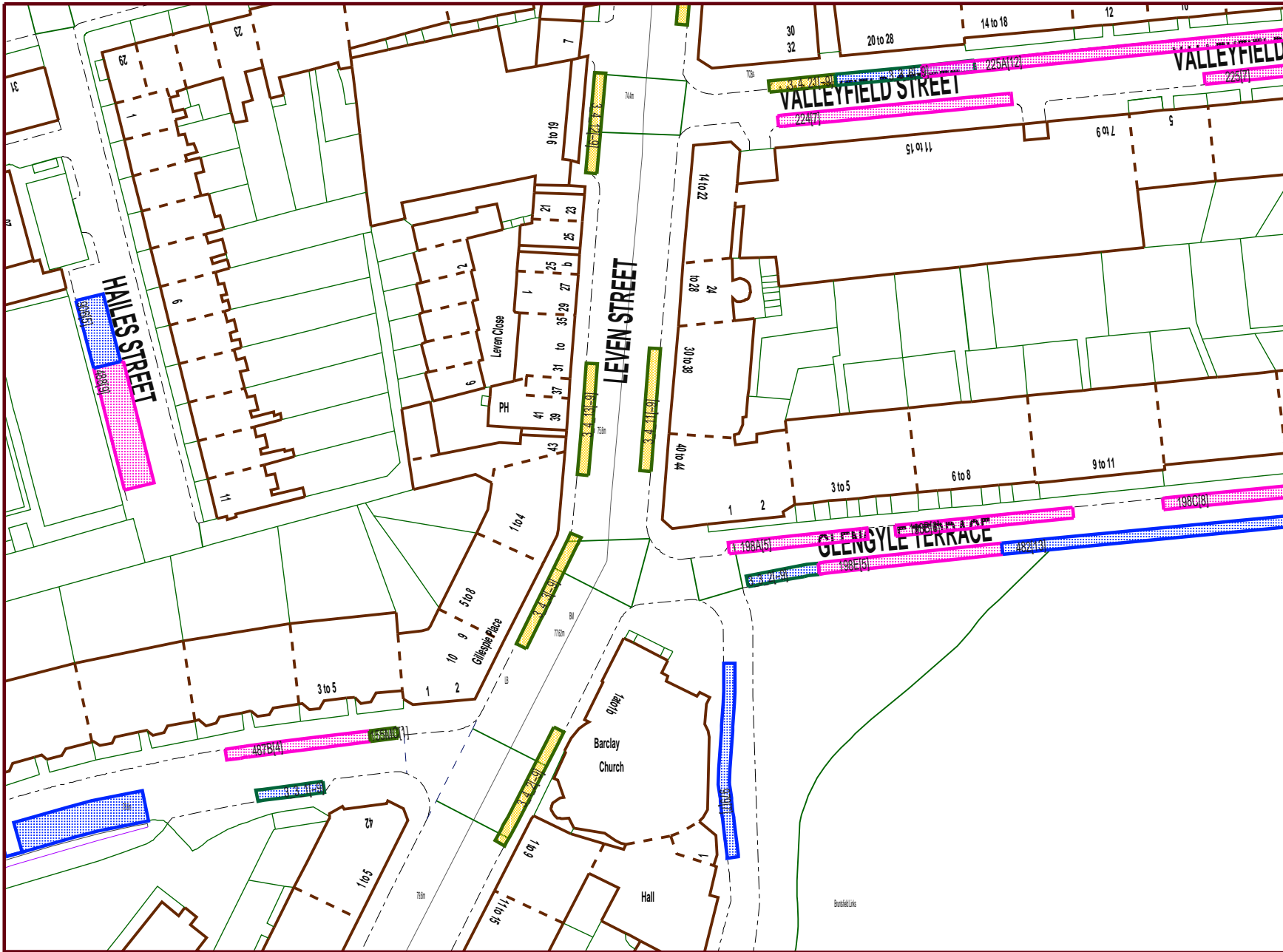
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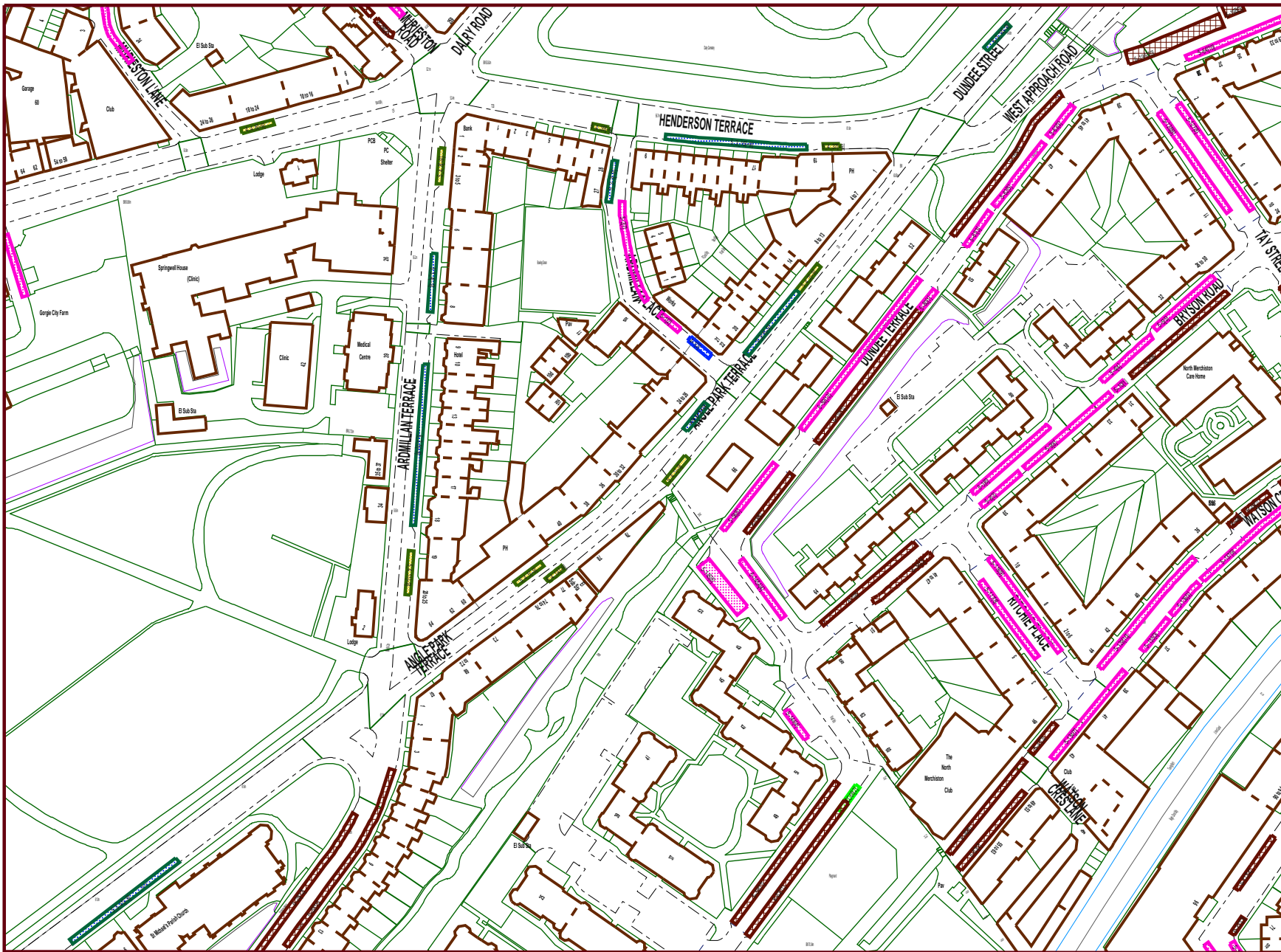


NOTES

Dark green: Greenways public parking places.

Streets included: Valleyfield Street, Glengyle Street and Gillespie Crescent.


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NOTES

Dark green: Greenways public parking places.

Streets included: Slateford Road, Angle Park Terrace and Dundee Street.

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Greenways Slateford Road Public Parking Places

1:2000



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